

## CHAPTER 1: TOP END

Posted on the Wildguzzi forum by Pete Roper: December 09, 2005:

**Contents: Tappet cover off, valve train off, open head nuts.**

*Pic 1-01:*

First remove the rocker covers, (8 bolts each side.) A 'T' Bar is really helpful here.



*Pic 1-02:*

Next the spark plugs, (Both of 'em.)



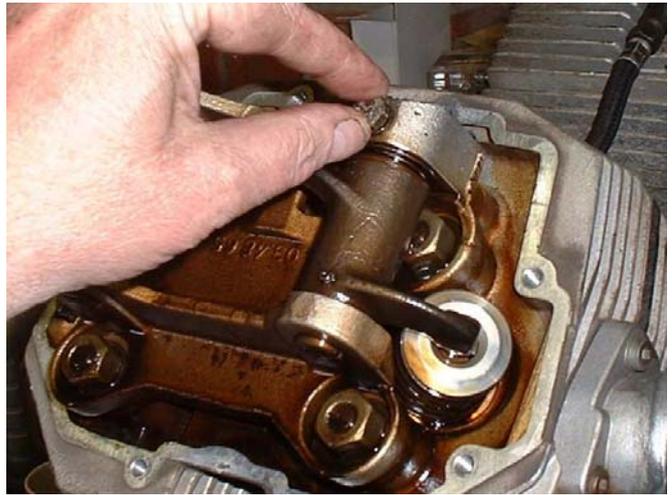
*Pic 1-03:*

Now use a key in the alternator bolt or a spanner on the nut on later models, or you can simply turn the flywheel, to get the cylinder you're working on to TDC compression. This can be checked by looking for the arrow on the flywheel and lining it up with the divot in the bell housing below the cylinder, (Sorry, no pics of that yet as this engine didn't have a flywheel on. Alternatively turn the crank clockwise with a key as shown and watch the inlet valve open and lose then turn the crank until both the pushrods will spin when you twist 'em, all they have to be is free to turn so they aren't pressing on the rockers.



*Pic 1-04:*

Next step? Remove the bolts and washers that secure the rocker spindles. This will allow you to use a screwdriver to push them out. They have a slot in the bottom end for this purpose!



*Pic 1-05:*

Once the spindles are out the rockers themselves can be removed. Note also that there is a spring and a brass washer at the top on roundfins and on squarefins a brass washer at the bottom of the rocker then another at the top, then the spring and then a steel washer betwixt the spring and the rocker carrier.



*Pic 1-06:*

Extract the pushrods.



*Pic 1-07:*

Using a 10mm allen key, (Preferably with a socket attachment like shown.) undo the blanking plug. This can be VERY tight. A good trick is to use a 6 Oz. hammer and blunt punch and tap around the plug to shock it and break the inevitable corrosion. Alternatively get a sodding great breaker bar and jar it loose!!!!

All these bits can be put in the parts tray handily provided by Moto Guzzi in the form of a rocker cover!!!!

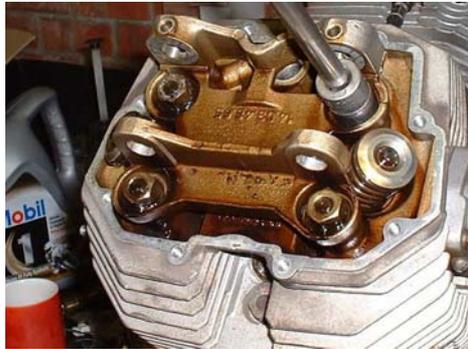


*Pic 1-08A:*

*Pic 1-08B:*

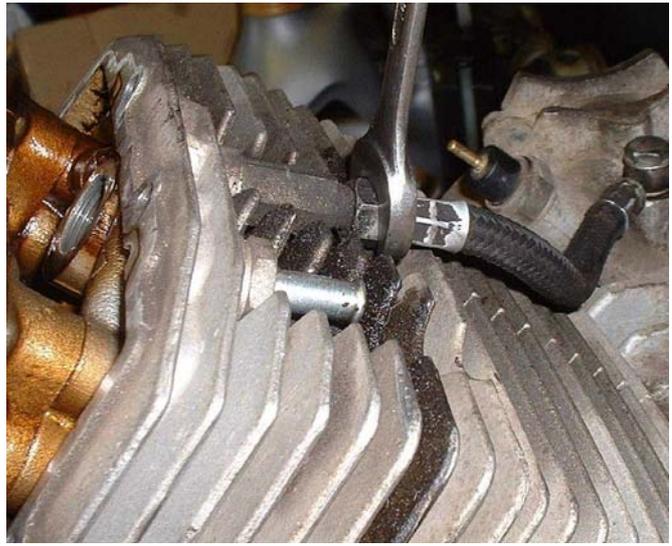
*Pic 1-08C:*

These three pics show the location of the head retaining nuts. The last one is the 'Sleeve Nut' that lives beneath the blanking plug we removed earlier. This, once undone, is usually a pain to get out. I just leave it in there until the head is off then tip it out. The nuts should be loosened in a crosshatch pattern, the order doesn't matter but I usually crack 'em off about half a turn first and then go round again and unwind 'em completely.



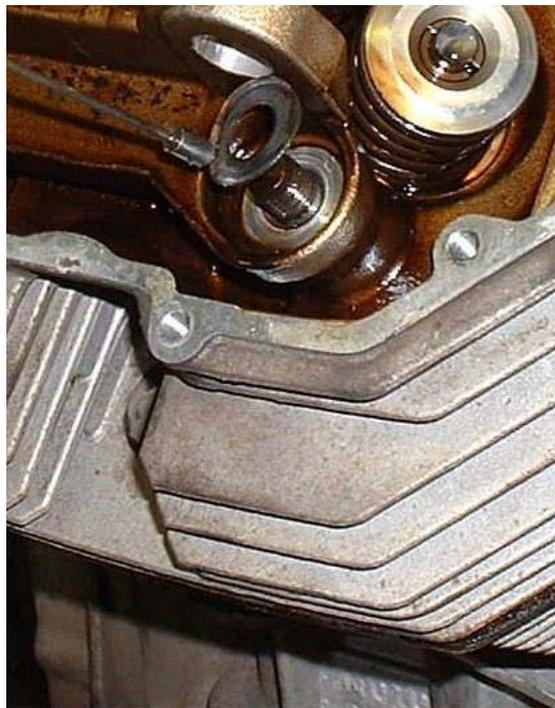
*Pic 1-09:*

At this point it's best to undo the oil feed pipe to the head. I usually forget until I try and reef the head off.



*Pic 1-10:*

Now, using a magnet on a stick or a small screwdriver lift out the washers underneath the retaining nuts. Don't forget that there is one on the stud next to the plug 'ole and note that there *\*should\** be a much smaller one under the sleeve nut. When you invert the head to get it out watch for it as it will probably drop out and disappear!



*Pic 1-11:*

Now the rocker carrier can be lifted off. Sometimes a bit of gentle prying with a large, flat bladed screwdriver is needed to get it to separate from the head. You do NOT try and pry between the carrier and the head itself. Lie a bit of rag or something on the side of the head and lever, GENTLY between the upper carrier and the head in a couple of places, not between the mating surfaces. It takes little to break it free, quite often just pulling the carrier up by hand will get it to lift OK but surface tension of the oil film that may be between the base of the carrier and the head or varnish build up, (The brown discolouration you can see on the head casting itself.) can make 'em a bit sticky.

