

GILARDONI PISTON INSTALLATION INSTRUCTIONS

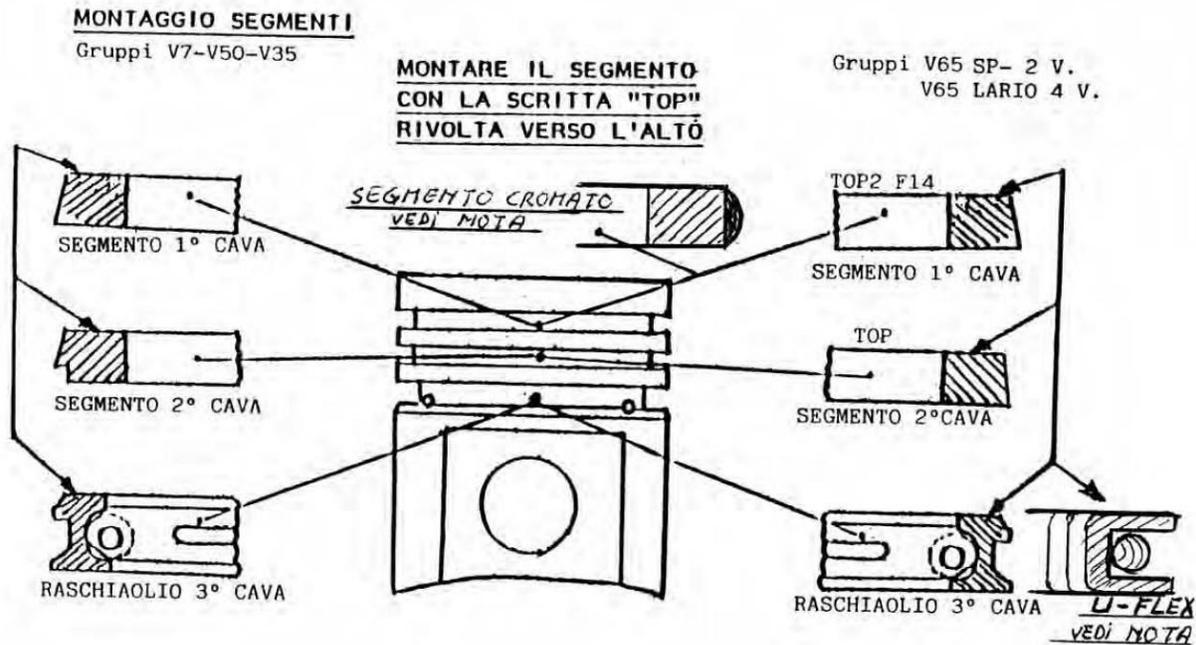
Translated by an Italian friend.

Important Notices

This group (Motorcycles) destined exclusively for sport competition has been studied and experimented with both workbench and on-road tests. Therefore we recommend for the best and most secure result to assemble the group (motor or cylinder head) as shown scrupulously following the assembly instructions.

Piston Ring Assembly

Groups V7-V50-V35	<u>Mount the Piston ring with the word "TOP" towards the top of the piston</u>	Groups V65 SP- 2 V. V65 LARIO 4 V.
Piston Ring 1 Top groove	<u>Chrome Segment (see note)</u>	Piston Ring 1
Piston Ring 2 Middle groove	<i>THIS TABLE FORMATTED TO MIMIC THE IMAGE BELOW</i>	Piston Ring 2
Oil scraper Ring 3 Bottom groove		Oil scraper Ring 3 <u>UFlex (see note)</u>



Note:

For a piston ring orientation different from that in the drawing mount the chromed piston ring in the first groove and the non-chromed in the second groove.

Connecting the Piston

Assembly and disassembly of the wrist pin into the piston to join it to the connecting rod should be done only by hand since the union is floating and does not require any force applied by a tool.

Attention: For type V35 and V50 motors the piston should be oriented in such a way that the selection letter or the arrow stamped on the head of the piston itself should face the front.

For motors of type V7 (*50 LM) the piston is positioned in such a way that the arrow on top of the piston is turned towards the exhaust side of the head.

For the Moto Morini 350 the piston should be positioned according to the Instruction Manual for the motor itself, that is, with the smaller valve relief on the exhaust valve and the larger diameter valve relief on the intake valve. For Guzzi V65 2 and 4 valve, the arrow is turned toward the exhaust valve.

Mounting the small lock-rings for the wrist pin

Place the lock-rings in their appropriate positions with the aid of lock-ring pliers. Be sure the lock-rings are completely in place.

Mounting the Cylinder

Lubricate the cylinder walls and piston rings with a little motor oil for inserting the piston into the cylinder. Use the appropriate tool to insert the piston rings. The only other piece needed is the gasket included.

Installing the Head

Tightening the head nuts, following a crossed order, should be done with a metric torque wrench following the prescribed tightening sequence.

- (motor type V35 and V50 Kgm. 3.8 - 4 (8 mm nut: 2.5 - 2.8) {18.1 – 20.3 foot pounds} the same for Guzzi V65
- (motor type V7 850 LM Kgm. 3.8 - 4) {27.5 – 29 foot pounds}
- (motor type Moto Morini 350 cc. Kgm. 2 - 2) {14.5 foot pounds}
- (motor type Ducati Cagiva 350 cc. Kgm 3.4 – 3.6) {24.6 – 26 foot pounds}
- (motor type Gilera Saturno 500-600 kdm. 4 - 4.2) {29 – 30.4 foot pounds}